

**A RESOLUTION AMENDING THE  
VISION 2015 COMPREHENSIVE PLAN  
FOR THE CITY OF KIRKWOOD  
TO ADD A SECTION TO THE PLAN AS AN APPENDIX ENTITLED  
"KIRKWOOD, MISSOURI DOWNTOWN URBAN DESIGN PLAN"**

**WHEREAS**, the City of Kirkwood is committed to retaining and enhancing the desirability and livability of its community and seeks to establish special considerations for the City's downtown area; and

**WHEREAS**, on the third day of April, 2003 the Planning and Zoning Commission unanimously approved the Vision 2015 Comprehensive Plan; and

**WHEREAS**, the Vision 2015 Comprehensive Plan contains goal statements which encourage the establishment of land use and site design ideas and strategies for future development in downtown Kirkwood; and

**WHEREAS**, in 2002, the Urban Design Plan Steering Committee began the development process of the Kirkwood, Missouri Urban Design Plan; and

**WHEREAS**, the Kirkwood, Missouri Urban Design Plan is a special area plan focused on the preservation and development of the City's downtown core; and

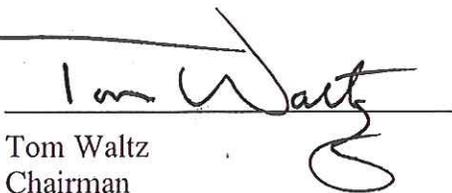
**WHEREAS**, the content of the Kirkwood, Missouri Urban Design Plan has been based on broad public input to establish site design policies, architectural design standards, and development strategies; and

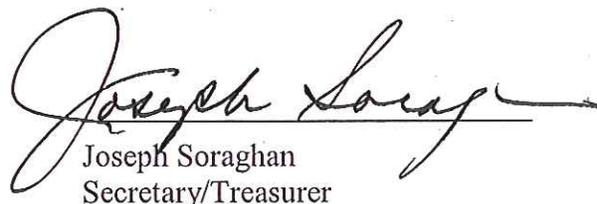
**WHEREAS**, a Public Hearing relative to this matter was held before the Planning and Zoning Commission in accordance with the Missouri Revised Statutes on the seventeenth day of September, 2003; and

**WHEREAS**, the Planning and Zoning Commission reviewed the Kirkwood, Missouri Urban Design Plan as presented by the Urban Design Plan Steering Committee, considered revisions to its content, and formed such recommendations into a final document.

**NOW, THEREFORE, BE IT RESOLVED BY THE PLANNING AND ZONING COMMISSION OF THE CITY OF KIRKWOOD, MISSOURI AS FOLLOWS:**

That the Planning and Zoning Commission of the City of Kirkwood, Missouri, under authority granted to it by the Revised Missouri Statutes, after a duly called public hearing, and after considering the views of all those who came before it, does hereby amend the City of Kirkwood Comprehensive Plan Vision 2015, dated April 3, 2003, to add a section to such plan as an appendix entitled "Kirkwood, Missouri Urban Design Plan", dated September 16, 2003, for the City of Kirkwood, Missouri.

  
Tom Waltz  
Chairman

  
Joseph Soraghan  
Secretary/Treasurer

A RESOLUTION AMENDING THE  
VISION 2015 COMPREHENSIVE PLAN  
FOR THE CITY OF KIRKWOOD  
TO DELETE SECTION 1.11, PLANTING DESIGN STANDARDS  
AND INSERT A NEW SECTION 1.11

WHEREAS, the Kirkwood Urban Forestry Commission has recommended the Downtown Urban Design Plan guidelines be revised regarding plant materials; and

WHEREAS, the Planning and Zoning Commission reviewed the recommendation and concurs with the Kirkwood Urban Forestry Commission.

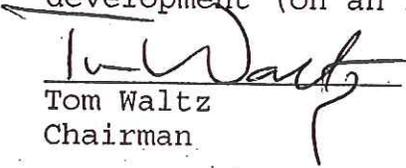
NOW, THEREFORE, BE IT RESOLVED BY THE PLANNING AND ZONING COMMISSION OF THE CITY OF KIRKWOOD, MISSOURI, AS FOLLOWS:

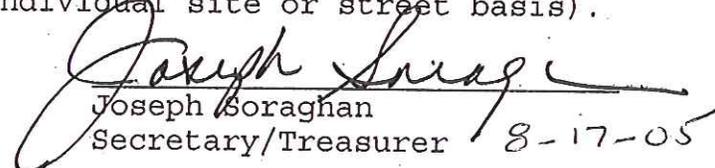
That the Planning and Zoning Commission of the City of Kirkwood, Missouri, under authority granted to it by the Revised Missouri Statutes and after considering the recommendation of the Kirkwood Urban Forestry Commission does hereby amend the City of Kirkwood Comprehensive Plan Vision 2015, dated April 3, 2003, by deleting Section 1.11 of the Downtown Urban Design Plan and inserting in lieu thereof the following:

Section 1.11 Planting Design Standards

Landscaped areas should accent the overall natural topographic conditions of the site and the character of the development on the site. Within landscaped areas provisions for lighting, benches, and other pedestrian related amenities should be made in order to create an overall system of landscape spaces accented by individual "garden-like" pieces.

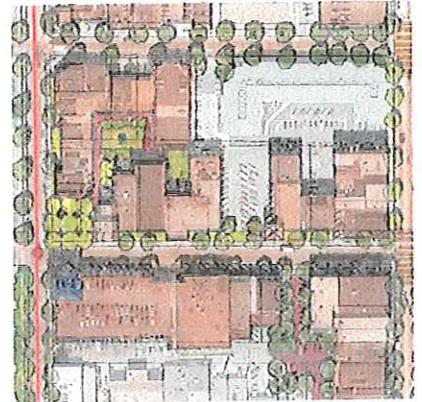
Trees recommended by the Kirkwood Urban Forestry Commission on their latest "Street Tree Selections for Kirkwood" are suggested to be used within the downtown area in combinations of at least two, but not more than three species for private landscape or street tree development (on an individual site or street basis).

  
Tom Waltz  
Chairman

  
Joseph Soraghan  
Secretary/Treasurer

8-17-05

# Kirkwood, Missouri Downtown Urban Design Plan



May 14, 2003

thirdland

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Holden Architects

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# Kirkwood Downtown Urban Design Plan

## Introduction

Kirkwood, Missouri is one of the oldest traditional suburbs in the region. In 1853, it was founded as a speculative suburban development, which promised to improve the quality of life for St. Louis City residents by using the railroad as a way to move beyond the confines of the central City in the region. The resulting community has provided a model for development elsewhere in the region, and it appears to be an inspiration today in exurban settings in the region.

However, over the years the City has incorporated and endured a number of changes as automobiles replaced the railroad orientation of the City, as it has grown beyond its original core. Recently, the City has examined its commercial areas more specifically to determine how it can maintain the old but incorporate new roles.

The City has one of the few suburban downtowns in the region. Over the years it has experienced economic pressures from regional development patterns: the introduction of automobile usage, development of regional malls, development of competitive neighbor communities, sprawl, abandonment of downtown structures and organizations. In order to sustain it in the 1970's, the City responded by incorporating current development approaches, i.e., big box and anchor stores like Target. Today, downtown is being looked at in a different way.

In 1985, the Comprehensive Plan provided specific strategies to support its still struggling downtown. After accomplishing a number of goals, the Special Business District (SBD), the organization responsible for promoting and improving downtown, created the "Downtown Vision Plan."

The **Downtown Vision Plan** states its vision for downtown:

*"Maintain the traditional downtown as the focus of the Kirkwood community for commercial, cultural and civic life and emphasize the historic character of downtown both in renovation and in compatible new construction."*

The City in response to other pressures and opportunities, commissioned in 1999 a market study of areas surrounding the new "Kirkwood Commons" power center to determine demand for additional retail, office and housing uses. Following that study, a market study for downtown was undertaken.

At the City's latest Strategic Planning session in Fall 2000, the City decided to undertake a new Comprehensive Plan process and a separate Urban Design Study for the downtown area including a land use component.

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The seven-member task force formed to steer the Urban Design Study represents the "Kirkwood Junction" (SBD), the Architectural Review Board, City Council, Landmarks Commission, a citizen architect and Kirkwood Area Chamber of Commerce.

## **Project Overview**

A task force of members of City commissions and organizations with interest in aesthetic and land use issues was appointed by the City Council to undertake the study with the help of experts in the urban design field. The task force is called the **Urban Design Team**. The City is simultaneously undertaking a comprehensive plan for the remainder of the City. The Urban Design Team and the Comprehensive Plan Steering Committee have coordinated their processes. The Kirkwood Urban Design Plan has focused on "Kirkwood Junction," the pedestrian-oriented downtown area located in the original 1853 historic boundaries of the City.

## **Background**

The urban design study area is the downtown area of the City of Kirkwood. The entire area is composed of 155 acres, bifurcated north and south by the Union Pacific Railroad Tracks, with the following boundaries: Bodley Avenue on the north, Taylor Avenue on the east, Woodbine Avenue on the south, and Clay Avenue on the west.

Particularly strong and vibrant residential areas including a number of condominiums, multifamily rental structures, churches and institutions surrounding the downtown area create a rich and complex mix of land uses within a relatively small core area. As part of the overall urban design study, these surrounding residential areas found primarily just outside of the study area, should be sustained and strengthened. The juxtaposition of residential, institutional and commercial properties and the varying patterns of connection created by each is an important issue addressed in the plan. While such juxtaposition can lead to some conflicts in terms of traffic patterns created by vehicles seeking to bypass activities and traffic on Kirkwood Road, the existing mix of residential uses with the predominantly downtown commercial, institutional and service uses is critical to the continued success, quality, and character of downtown Kirkwood.

The downtown area is primarily zoned B-2 Downtown Business with some areas that were developed under the B-4 Planned Commercial Development zone. Multifamily zoning currently rings downtown, although the area also includes institutional uses and single-family housing. A Special Business District was created in 1975 for the downtown area. Most of the study area is also included in the Special Business District (SBD). An appointed advisory commission of business and property owners in the district governs it with the approval of City Council. The SBD has a dedicated tax base and bonding authority. Current development activities suggest a robust stage of urban evolution is now in progress, with the likely outcome

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to be an increase in the scale and qualities of future projects. The urban design plan is intended to anticipate and guide this evolution.

### **Public Participation in Plan Development**

In addition to the appointment of the Design Team members, the City solicited public participation throughout the UD Plan process including a Visioning Meeting, and several opportunities to review and comment on the initial and final draft development of the plan. The two-day Visioning Meeting – an active charrette design process – was used during the preliminary stage of the plan process to provide interested individuals and groups an opportunity to participate directly in the making of the plan concepts. Public preferences regarding scale, type, style, and uses were documented in order to guide the plan contents and intentions. Following the hands-on planning work performed by citizens, the Design Team solidified and refined the concepts and re-presented a draft plan for additional comment by the public along with information generated through citizen surveys and visual preference scores. These public meetings were used to guide the process through further refinement, and led to the development of the final draft plan that was again presented to the public for comment. Based on the broad and significant public support of the plan concepts, intentions, and requirements for future development in the downtown area, the Design Team prepared the following UD Plan for adoption by the City.

### **Intentions of the Plan**

The plan is intended to create a community environment that is vibrant, livable, and desirable. In order to build and maintain Kirkwood as such a place, measurable means of achieving these intentions are necessary. The Kirkwood Downtown Urban Design Plan (UD Plan) intends to:

1. **guide development** towards identified qualities and land uses for the community
2. provide **specific, enforceable and measurable guidelines** for Plan achievement
3. provide **models and design examples** to underscore specific goals
4. provide the City with an objective basis to **review** proposed development plans

### **Land Use and Urban Design Objectives**

1. **Establish land use and site design strategies for future development in downtown**
  - Create complementary commercial destination areas through appropriate land use strategies
  - Enhance/ strengthen the downtown commercial areas as a retail-oriented, mixed- use "destination" for the region
  - Encourage the development of housing options that support balanced day and night-time use of downtown

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- Maintain and enhance cultural and civic uses within downtown
  - Establish a parking strategy for downtown that supports proposed land uses

**2. Develop sustainable relationships between the downtown commercial core and surrounding residential areas**

- Establish boundaries and development limits for the commercial core areas to protect surrounding residential neighborhoods
- Enhance connectivity and increase opportunities for pedestrian and bike traffic to easily access downtown destinations from surrounding neighborhoods
- Address vehicular traffic impact on surrounding neighborhoods

**3. Establish design strategies and guidelines that support the concept of pedestrian oriented, and appropriately scaled development**

- Provide conceptual design strategies and objectives for downtown that will result in the development of high quality structures, places, spaces, and connective networks on an individual project basis
- Craft design guidelines that establish minimum criteria for architecture, landscape architecture, and site planning that contributes, through the development of each project, to the urban design and land use objectives for downtown.
- Establish a coherent Urban Design plan and framework within which all future downtown development proposals may be evaluated

**Plan Use and Application**

The UD plan is intended to serve as a planning tool which will guide all future development within the plan study area. Overall goals for the plan are attained through implementation on an individual project basis where the sum of such projects results in the comprehensive fulfillment of the planned environment for downtown Kirkwood. All projects for development and/ or redevelopment located within the downtown plan area shall be reviewed for conformance to the UD plan, including its land use concepts and architectural guidelines.

**Plan Implementation Procedure**

In its current form, the UD Plan is to be considered as an advisory document, but should be utilized to the greatest extent possible for the review of development projects which will impact downtown Kirkwood. The UD Plan is intended to be utilized in conjunction with the City's comprehensive plan and zoning code. The language and statements in the UD Plan provide standards, policies, scenarios, and recommendations that may be codified as determined by the City. Projects will be reviewed by the City for conformance to the UD plan, including all applicable land use and design guidelines. The UD plan is envisioned to be implemented by the City through a project review process that includes an initial land use and design

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analysis by Staff; followed by the appropriate reviews of the Planning and Zoning Commission, Architectural Review Board, and City Council. The City is strongly encouraged to determine and publish current procedural requirements for review and approval of projects that are part of the Downtown Urban Design Study Area. The following is a recommended list of submittal requirements for development projects within the study area. As the significance and size of projects will vary, any or all of these may be required by the City upon preliminary review of the project:

A preliminary development plan shall be required for any use or circumstance requiring a development plan as identified by the City. Requirements for plans submitted for UD plan compliance review include:

General Submission Requirements. The applicant shall submit to the City 4 copies of the proposed development plan, or such other number required by the City. The proposed development plan, detail requirements, and required maps shall be provided in paper and in compatible electronic format unless otherwise waived. The preliminary development plan shall be accompanied by all general application requirements and shall include:

Project Statement:

A written narrative of the project outlining its significant features, proposed uses, potential impact on neighborhood surroundings, and any anticipated community benefits (provision of public space, architectural quality, sales tax estimates, etc.)

Legal description:

A written legal description of the property included in the proposed development.

Detail requirements: the property to be included in the proposed development, plus the area within 200 feet of all property boundaries shall be shown, including the location of existing and proposed:

Buildings and other structures;

Property lines with ownership delineated;

Parking areas, loading spaces, drives and walkways;

Screening and landscaping, including location, height and materials;

Drainage patterns and structures, including location and size of any culvert, sewer, ditch or other drainage structure;

Public streets and curb cuts;

Fences and walls, including location, height and materials;

Signage, including conceptual graphics and elevations;

Easements;

Utilities, including preliminary location of water, sewer, gas and electric facilities;

Any areas for public use and/or dedication;

Identification of noise generation locations;

Existing topography with contours at 2-foot intervals;

Delineation of any land areas subject to 100 year flood;

Written & graphic scales accompanying all maps and drawings;

North oriented to the top or to the left side of the sheet with graphic arrow on all plan drawings;

Boundaries, exterior bearings and dimensions clearly shown.

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Architectural elevations, sections, physical models, and material conditions.:

Scaled elevation views shall be prepared with proposed colors to accurately describe all faces of proposed buildings and landscape design and their proposed height and massing relationship to all surrounding developments and/ or buildings;

Scaled section views shall be prepared indicating the proposed development and height and massing relationship to all surrounding developments and/ or buildings;

Consider a physical model at 1/8 " = 1' - 0" scale including all proposed building, site, and landscape elements;

Material sample or mock up boards shall be submitted depicting the, size, texture, color and exterior construction materials of the buildings proposed.

An aerial vicinity map:

The City will provide a 1" = 100' - 0" scale aerial map for the applicant to show the project location and a 1,000 foot radius including: names of major roadways, public streets and their classification, i.e. local, collector, arterial;

Surrounding uses and adjacent properties;

Existing streams, bodies of water, and watersheds.

Schedules.

A schedule shall be included indicating total required and provided: floor area, dwelling units, land area, parking spaces, land use intensity, hours of operation of the business, and other quantities specified in the appropriate zoning district regulations.

Supplemental information:

Such other information as may be requested by the Director as may be reasonably needed or appropriate to adequately review the proposed development and any potential impacts.

Phases of development

Phases of development must be shown on the preliminary development plan, if applicable. If the development will occur in phases, the applicant shall submit a development plan that also displays the entire development at the completion of all phases. The phased development shall have the phases clearly outlined with expected dates for beginning of construction and date of completion of construction. No building permit shall be issued for any phase of development until a final development plan for that phase is approved, in accordance with the provisions of the plan. Once phased development plans have been approved, any modification to any phase of development may be subject to full review and approval by the City through the same process as outlined in this section.

Statement of need for deviations from the UD plan requirements:

A narrative statement that specifically explains the need for any deviation from applicable UD plan requirements shall be submitted with the application. Requests for deviations from any other requirement or ordinance established by the City that is not a part of the UD plan requirements shall be submitted to the City under separate application.

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## Urban Design Development Plan and Special Area Details

The following illustration identifies the areas and extent of potential developments that may be expected to occur within the study area over a ten to fifteen year time frame. The plan illustration is intended to articulate the qualities, character, scale, and general outcome of the implementation of the Downtown Urban Design Plan including design guidelines and requirements found in Section 1.0 Site Design, and Section 2.0 Building Design.

Where the Plan indicates new or modified building forms and footprints, such illustrated development is not intended to indicate specific building proposals. Rather, the illustrated changes represent a likely development scenario in terms of building site coverage, building to public realm relationship, potential scale, and implementation of design guidelines such as pedestrian and bicycle circulation, streetscape development, and plazas and public spaces. The Urban Design Development Plan is to be used in conjunction with the supporting specific design guidelines in order to provide the City and its residents with both a comprehensive perspective of high quality and realistic downtown development opportunities, and the specific means necessary to implement such a perspective.



Buffer landscape between commercial and residential uses.  
(Sec. 1.6 Setbacks and Enhanced Buffers)

Alternative landscape for parking lot with space limitations.

Detail Area - Library Square

Structured Parking.  
(Sec. 2.9 Structured Parking)

Open/Civic space themed landscapes including bosques with uplighting intended as use areas for vending, public use, and social gatherings.  
(Sec. 1.7 Site Lighting; Sec. 1.10 Plazas/Public Spaces)

Enhanced pedestrian circulation and pavement at vehicular crossings.  
(Sec. 1.9 Pedestrian/Bicycle Circulation)

Enhanced Landscape and Farmer's Market as core open space and public use area.  
(Sec. 1.10 Plazas/Public Spaces)

Detail Area - Civic Plaza Area

Building footprint representing general site development with implementation of UD/Site Design Guidelines. Representations of building footprints are approximations of maximum development potential.

Plaza/Pedestrian space with water feature - can be combined with vehicular drop off area.  
(Sec. 1.13 Fountains and Water Elements)

Micro retention designed to create pedestrian circulation and use space.  
(Sec. 1.5 Storm Water Collection)

Parking lot landscape.  
(Sec. 1.4 Parking Lots, Driveways, and Service Ways)

Enhanced landscape screen at surface parking lots and street edge.  
(Sec. 1.4 Parking Lots, Driveways, and Service Ways)



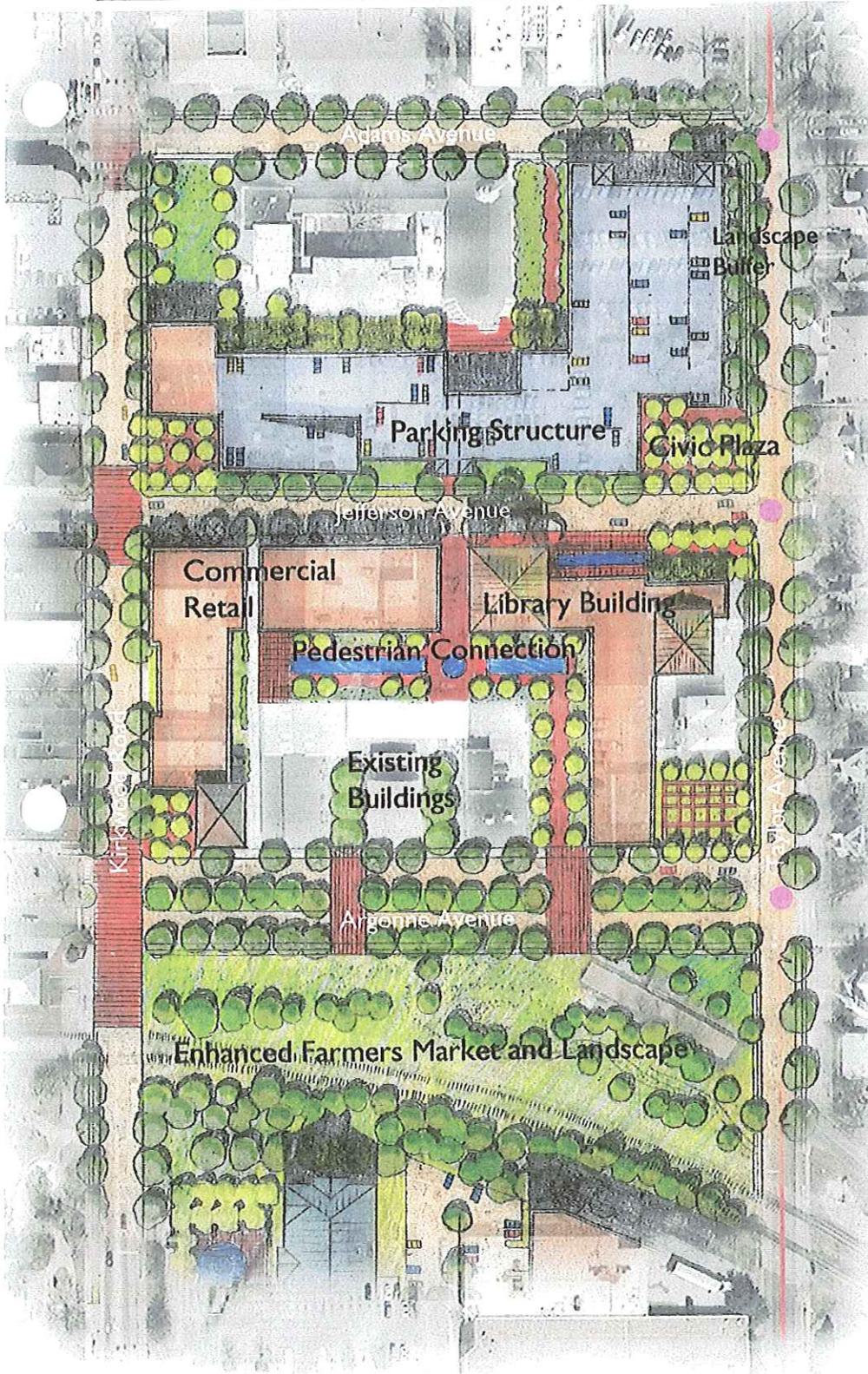
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**Figure 1 Urban Design Development Plan**

Two key areas selected for illustration include several possible joint public and private development opportunities. Such developments provide for potential expansion of downtown businesses that are similar in scale to current uses and which are necessary to maintain economic vitality and community qualities of the downtown area.

The first, an area between Adams Street to the north, Argonne Avenue to the south, Kirkwood Road to the west, and Taylor Avenue to the east, is illustrated in Figure 2. The area was selected to identify significant opportunities for joint development between the existing Public Library located on Jefferson that wishes to expand its facilities, and the First Presbyterian Church of Kirkwood, which may also wish to expand its facilities. A development model that promotes pedestrian oriented, compact and moderate density development is envisioned for the site. The model includes up to three levels of building height, which is necessary to achieve such expansions, create additional private commercial development, promote the qualities and character of a walkable downtown district, and to provide off-street parking alternatives for existing businesses and uses that lack adequate parking.

Figure 3 illustrates a second potential mixed-use development located south of Madison Avenue, north of Monroe Avenue, east of Clay Avenue, and west of Kirkwood Road. Similar to the description of the detailed area in Figure 2, this area appears to be a significant opportunity for development that would be anchored around the civic buildings to the north of Madison Avenue. It would provide a major interior-block linkage between existing commercial development to the south and the civic core and central downtown area to the north. The mixed-use model for structured parking with ground floor commercial uses would support development envisioned in this area located mid-block on the south side of Madison Avenue. Expected uses could include additions to existing commercial development fronting Kirkwood Road and Monroe Avenue, and residential uses above ground floor commercial on Clay Avenue.



This Plan is conceptual in nature and represents the potential development, in density and scale that may recur in downtown Kirkwood by the year 2015.

- Parking Structure
- Retail Commercial
- Library
- Pedestrian Connection
- Park-like Green Space and Farmers Market

**Figure 2 Library Square Concept Plan**



This Plan is conceptual in nature and represents the potential development, in density and scale that may recur in downtown Kirkwood by the year 2015.

- Landscape Park Areas
- Plazas
- Pedestrian/Bicycle Links
- Park Structures
- Offices
- Mixed Uses
- Residential on Downtown Edges

**Figure 3 Civic Plaza Area Concept Plan**

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The Urban Design guidelines are described in the following two sections that address fundamental Urban Design criteria: Site Design, and Building Design. Issues addressed in the guidelines include:

## **1.0 Site Design**

- 1.1 Allowable Land Uses
- 1.2 Building Height, and Impervious Coverage
- 1.3 Yards and Setbacks
- 1.4 Parking Lots, Driveways and Serviceways
- 1.5 Storm Water Collection – Micro-retention Systems as landscape elements
- 1.6 Setbacks and Enhanced Buffers at Land Use Transition Boundaries
- 1.7 Site Lighting
- 1.8 Streetscape
- 1.9 Pedestrian/ Bicycle Circulation
- 1.10 Plazas/Civic Spaces
- 1.11 Planting Design Standards
- 1.12 Site Signage
- 1.13 Fountains and Water Elements
- 1.14 Service, Loading, and Utilities

## **2.0 Building Design**

- 2.1 Building Massing, Scale
- 2.2 Consistent Quality Materials
- 2.3 Building Details
- 2.4 Lighting – Interior/ Exterior
- 2.5 Signage/Building Addresses
- 2.6 Mechanical and Service Screening
- 2.7 Accessibility
- 2.8 Relationship of Building to Parking and/ or Street
- 2.9 Structured Parking
- 2.10 Historic Preservation

The land use plan includes a range of land uses for the downtown area that are intended to maintain a high level of quality, economic vitality, and the scale and type of development that residents of Kirkwood and previous market studies have stated are important to sustain the community. Several key future land uses include structured parking with ground floor commercial use that would serve surrounding existing and new development, mixed-use development defined as having ground floor commercial uses with residential uses above, and additional civic/ open space and institutional development. These uses, combined with traditional uses such as single family residential, multi-family residential, and a range of commercial land uses that support the concept of a full-service, working, and living community are illustrated in Figure 4.

The land use plan is an important part of the Downtown Urban Design Plan in terms of designating the types, locations, and relationships of various land uses proposed for the downtown area. Allowable Land Uses in the Downtown Area include:

**Structured Parking** – parking on multiple levels with ground floor occupied by commercial uses, or in the case where such structures occupy frontage on Taylor Avenue, Bodley Avenue, or Clay Avenue, a minimum 20-foot setback with full landscape screen shall be required.

**Existing Residential Single-Family** – such residences shall maintain the general scale and character of the residential properties within the surrounding one block area. Such Single Family residences shall be exempt from the specific design guidelines in the UD Plan.

**Residential Multi-Family** – where possible, such residences shall be designed to create the maximum density allowable under the current zoning ordinance

**Mixed- Use Development** – (Commercial/Residential) ground floor commercial uses with residential uses constructed above (residential uses to constitute a minimum of 60% of building square footage).

**Commercial-Office** – professional and general office use

**Commercial-Retail** – grocery, restaurant, clothing, furniture, or other such retail use

**Commercial-Service** – fuel stations, laundries, or other service operations

**Institutional** – churches, libraries, government offices, schools, and community services

**Civic/ Open Space** – parkland, plazas, recreational land, or other public lands



**Figure 4 Future Land Use Plan**

\* Indicates locations where structured parking could be appropriate in conjunction with future development. However, any parking structure

## 1.2 Building Height, Impervious Coverage

Building heights, building site coverage, and total impervious surface coverage limits are outlined in the following table for the UD plan area. Additional building height may be considered as part of the application review where the applicant exhibits extraordinary design and/ or other community benefit as determined by the City. The chart below illustrates height and coverage recommendations which correspond to allowable land uses and their locations.

<b>Land Use Designation</b>	<b>Maximum Building Height</b>	<b>Building Site Coverage (a)</b>	<b>Impervious Surface (b)</b>
Parking Structure	40' Roof use allowed	90%	95%
Residential-Multi-Family	40' and 3 stories	75%	85%
Mixed-Use Development	40' and 3 stories	80%	90%
Commercial-Office	40' and 3 stories	70%	80%
Commercial-Retail	40' and 3 stories	70%	85%
Commercial-Service	35' or 2 stories	60%	80%
Institutional	50'	75%	85%
Civic/ Open Space	NA	NA	60%

### NOTES:

- a. Building Site Coverage refers to the building footprint, or the maximum site area that any building footprint including accessory structures may occupy as a percentage of the entire site area.
- b. Impervious Surface refers to the maximum "paved surface" area, including building coverage, parking lots, service drives, driveways, service pads, etc. that may occupy the site as a percentage of the entire site area.

### 1.3 Yards and Setbacks

All developed parcels are to maintain minimum setbacks identified in the following chart. Provisions for landscape amenities and lighting should be made in order to enhance the character of these spaces.

Plan Symbol	Land Use Designation	Side/ Rear Yard	Front Yard
<b>RS</b>	Residential-Single Family	15' / 40'	Average front yard setbacks within 300' , same side of street
<b>RM</b>	Residential-Multi-Family	25' / 40'	25'
<b>MXD</b>	Mixed-Residential and Commercial	0' / 20'	12'
<b>CO</b>	Commercial-Office	0' / 20'	0' (inc. Streetscape)
<b>CR</b>	Commercial-Retail	0' / 20'	0' (inc. Streetscape)
<b>CS</b>	Commercial-Service	0' / 25'	25'
<b>IL</b>	Industrial-Light	15'	20'
<b>IN</b>	Institutional	0' / 25'	4'
<b>OC</b>	Open/Civic Space	NA	NA
<b>PS</b>	Parking Structure	0' / 20'	4' Primary Street 20' Secondary Street

### 1.4 Parking Lots, Driveways and Serviceways

Vehicular access points (curb cuts) to individual parcels should be limited to one per parcel, or no more than one per 200' (two-hundred feet) of frontage on Kirkwood Road within a single parcel.

In order to create simple, easily recognizable access to development within the downtown area, the plan encourages sharing access from Kirkwood Road with adjacent parcels, where possible. Driveways and parking areas should be designed to accommodate efficient vehicle stacking during peak periods, based on a specific traffic analysis prepared for the proposed development. All site plans showing such driveways and parking areas are to be simultaneously submitted for review and approval by the City and the Missouri Department of Transportation.

Surface parking areas should not exceed the number of parking spaces required in the plan. Proposed parking spaces that exceed the number of spaces required in the plan shall not be permitted without approval of the City.

Where possible, parking lots and service alleys should not dominate the street frontage. Building walls and entries, landscape conditions, and pedestrian areas should comprise a minimum of 60% of the street frontage.

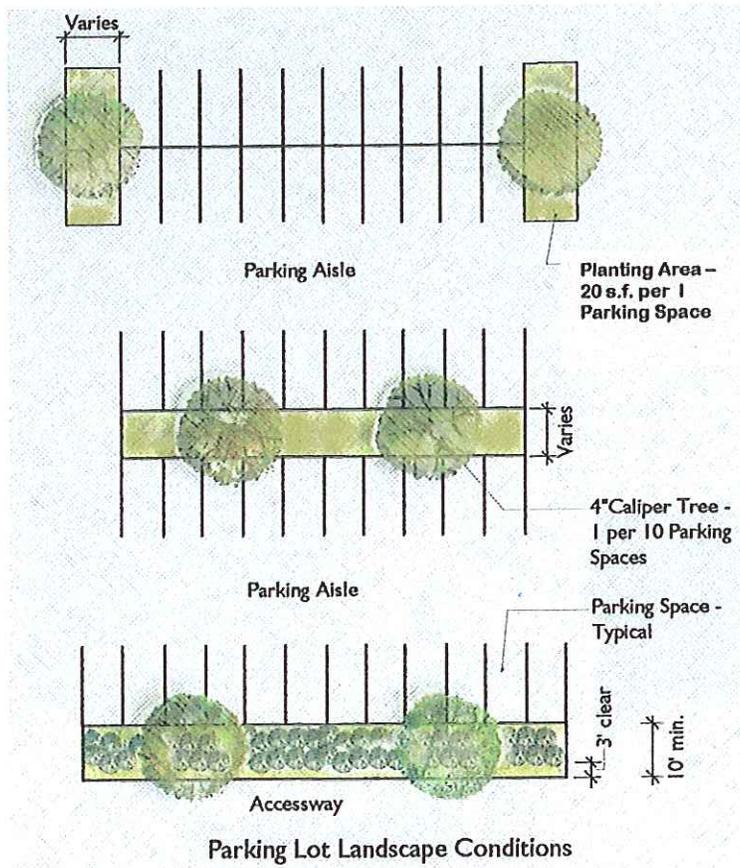
The City should review and update its parking requirements to reflect the latest standards for downtown developments. Parking requirements may be modified on a project basis with supporting traffic engineering/ parking plan approved by the City. Off-site parking may be allowed by the City under specific and long-term agreements between an Applicant and an off-site parking provider, including the City.

Parking lots shall be screened to a height of 30 inches from public streets with solid walls or to a height of 3 feet with hedges to be selected from the Forestry Commission approved plant list. Canopy trees from a City approved tree list shall be planted 30 feet on center within the screen hedge line. An additional row of approved canopy trees shall be planted 35 feet on center, inside the public street curb line fronting the parking lot. The purpose of this double row of trees is to provide screening in addition to the required hedge, shade for sidewalk areas, and aid in the creation of a pedestrian-scaled environment.

Parking lots shall be marked at the primary entries with an approved standard color and scaled sign that is affixed to the required site wall. The owner may post up to one sign per parking lot entry.

In addition to the above guidelines, parking areas shall be landscaped using the following standards:

For surface parking lots, not less than 20 square feet of interior landscape planting area shall be required for every parking space approved for development. Plantings required along the perimeter of the parking area for screening shall not be considered as part of the interior landscaping requirement. Within the required interior parking lot landscape, a sprinkler system shall be installed and a minimum of one 4 inch caliper tree shall be planted for every ten (10) parking spaces constructed, and shall be dispersed throughout the parking area to provide shade and cover. A single tree species should be used for each parking lot. The species selected should be indigenous to the region, or of a horticultural variety that is identified as a USDA Plant Zone 4 or 5 materials.



The interior dimensions of any planting area or planting median shall be sufficient to protect the landscaping materials planted and to ensure proper growth, in no event shall any such area be less than 60 square feet in area or 4 feet in width.

For every 25 parking spaces located in each parcel, 400 square feet of plaza or outdoor pedestrian space shall be required within the same development parcel. Such a space shall conform to the guidelines found in section 1.10 Plazas/ Public Spaces, and should be designed to compliment the architectural design and project uses. Projects containing less than 25 parking spaces and parking structures shall be exempt from this requirement.

## 1.5 Storm Water Collection

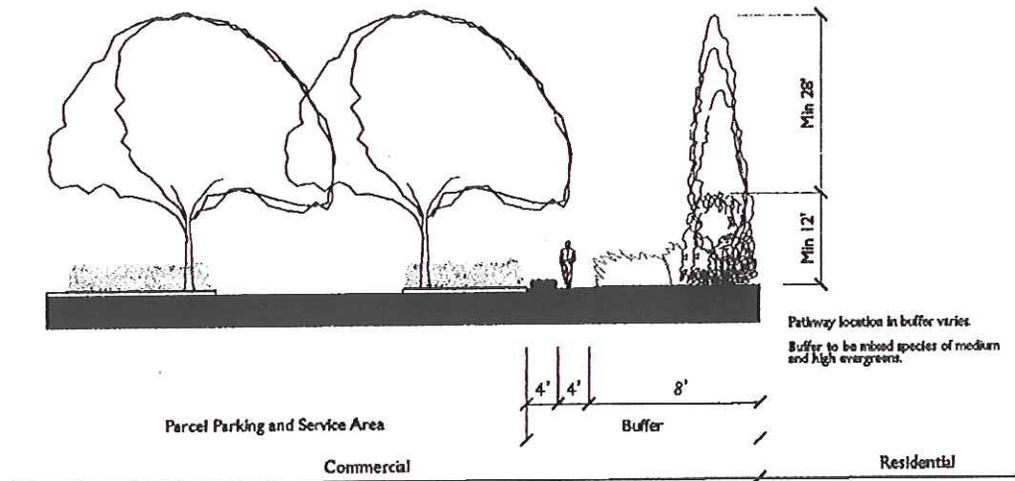
Water features are encouraged as part of any micro-retention or on-site storm water management collection areas are encouraged as part of each parcel development. In conjunction with the development of these collection areas, amenities such as lighting and landscape planting as well as pedestrian plazas, areas, and seating should be developed. In time as each parcel is developed, these detention areas are intended to create a comprehensive system of pedestrian circulation ways and open space elements. Developers are encouraged to consult with adjacent property owners to coordinate their efforts in this regard.



### Storm Water Retention in Pedestrian Plaza Area

## 1.6 Setbacks and Enhanced Buffers

A 16' wide landscape buffer should be implemented on all commercial developments where they abut residential or other non-commercial zones.



**Landscape Buffer at Parking Edge**

## 1.7 Site Lighting

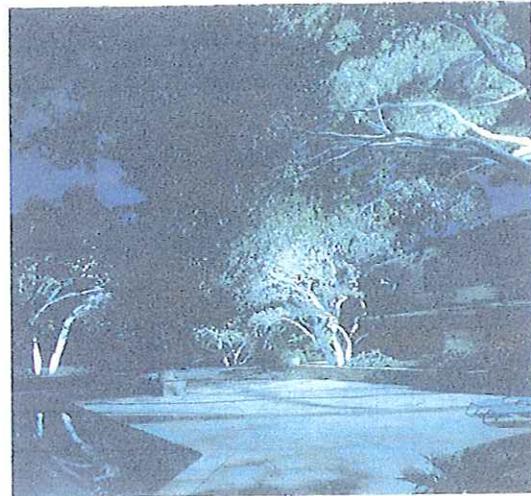
These guidelines encourage the use of landscape lighting to achieve the required illumination within parking lots and environs. The use of "moonlighting," "silhouette lighting," or "pathlighting" techniques are all suggested for use in these areas, and may be used in combination with more conventional area lighting usually provided by parking lot and roadway lights (mast-carried luminaires, not to exceed 18' in height with cut-off devices to minimize light spillage to off-site areas). Metal halide or mercury vapor lights are preferred, and in no cases shall high-pressure sodium lights be allowed.

A maintained average minimum required illumination for parking lots is 1.0 footcandles (fc). Pedestrian walkway lighting shall provide a minimum maintained average illumination of 2.0 footcandles (fc). In all cases, it shall be the responsibility of the owner to propose, create, and maintain adequate levels of illumination for proper safety and use of the site.

Site lighting used for area illumination and architectural lighting should be indirect in character (no light source visible). Indirect wall lighting, overhead down-lighting, or interior illumination, which spills into the landscape, is encouraged. Architectural lighting should articulate the particular site and use, as well as provide the required functional lighting for safety and clarity of pedestrian movement.

Lighting should highlight, compliment, and reinforce landscape and architectural design as focal points of interest.

Lighting should not impact adjacent residential neighborhoods, or traffic movement on adjacent roadways.



**Example of Site Lighting in Pedestrian Areas and as Landscape Focus**

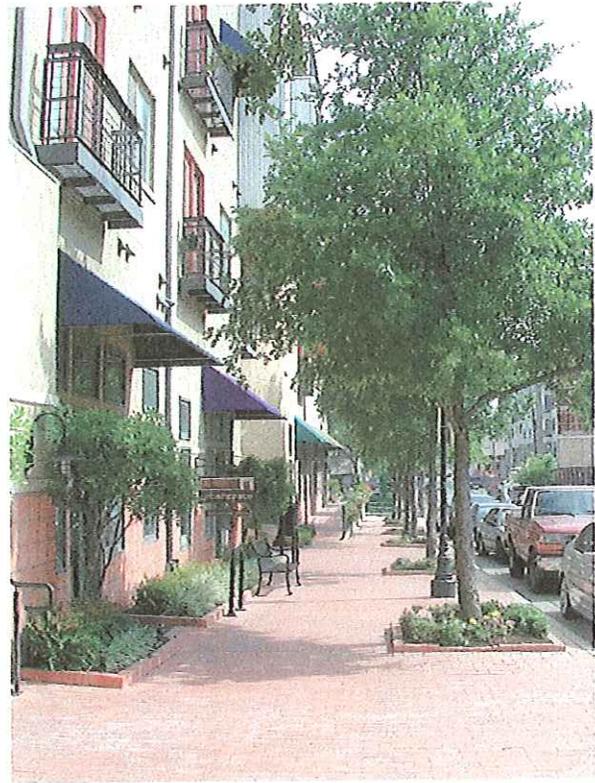
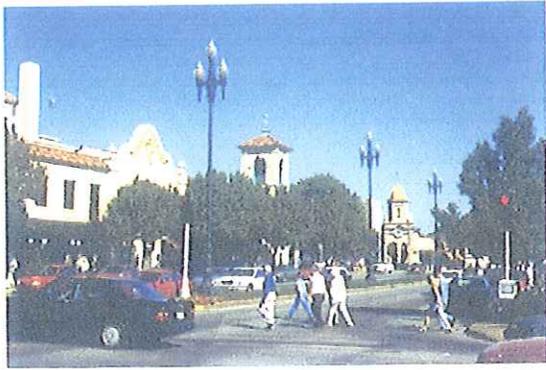
## **1.8 Streetscape**

All streets within the plan area shall be designed to provide for street trees planted 35 feet on center (calculated on total street frontage including width of accessways, etc.), sidewalks, and hedge plantings that are intended to create enhanced linkages to and from the surrounding neighborhoods. Provisions for sidewalks, plaza/public spaces, street trees, lighting, and landscape amenities should be provided as part of each private development plan submitted for approval. The intent is to provide an attractive street frontage to all parcels and to ensure the vitality of the street corridor through pedestrian accommodation and use.

All streets should include a minimum 4-foot wide sidewalk with a minimum 4 foot wide streetyard to allow for street tree and hedge planting. However, where sidewalks abut building frontage, this standard guideline shall be increased to 10 feet in total width and planting of street trees should occur within a grate-covered tree planting pit that is at least 4 feet in diameter, or 4 feet x 4 feet square.

For street tree species selection, see section 1.11 Planting Design Standards.

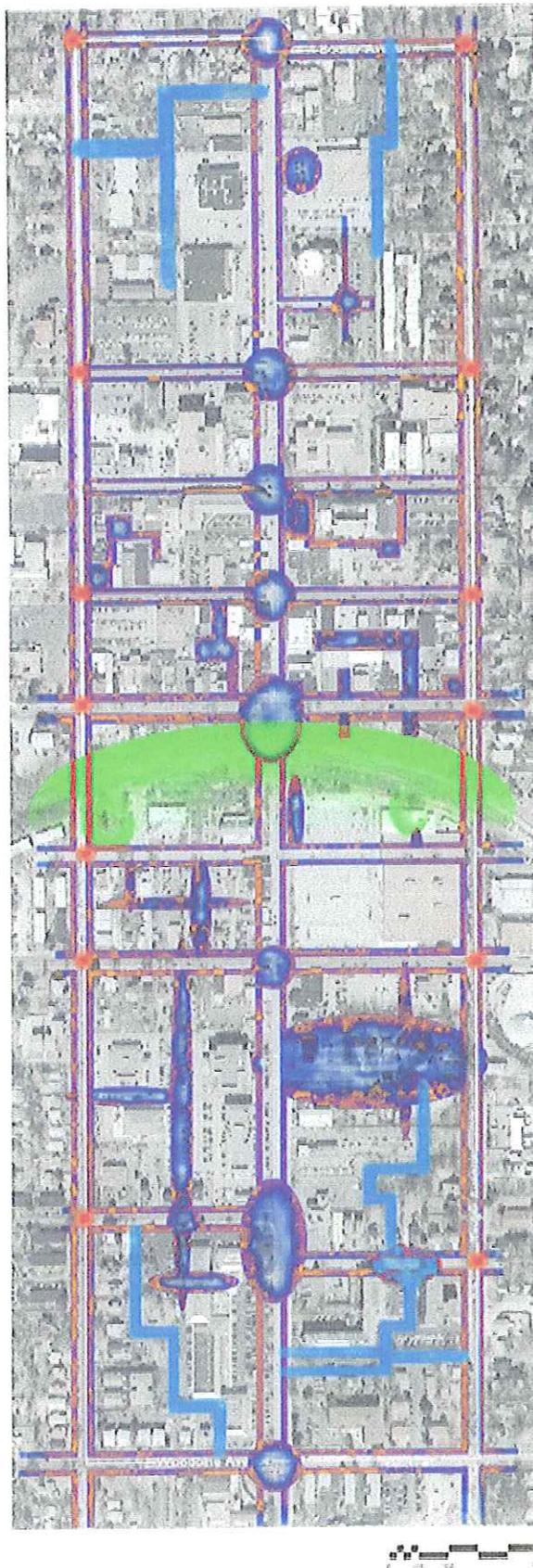
Along both sides of all public streets within the downtown, a standardized street light plan should be implemented to provide minimum safety and nighttime use standards. The City shall determine a standard fixture for use in all public projects, and for inclusion by private development where required as part of project streetscape development requirements.



### **Standard Streetscape Lighting Character and Street Tree Standard Planting**

## **1.9 Pedestrian/ Bicycle Circulation**

All parcels are to provide adequate parcel-to-parcel pedestrian access, in order to facilitate the safe and efficient movement of pedestrian traffic throughout the downtown. Along pedestrian routes, provisions for landscape amenities such as benches, lighting, trash receptacles and landscaping should be included as part of the site design submitted for approval by the City.



- Traffic Calming Measures and Devices
- ⊕ Public Circulation Ways and Nodes
- Railroad and Depot Park and Landscape
- ⚡ Private Connections Linking to Public Circulation Ways

Pedestrian/Bicycle Circulation

**Figure 5 Pedestrian/Bicycle Circulation Plan**



### **Pedestrian Crosswalks and Representative Material Conditions**

Approved materials for pedestrian circulation routes and areas include:

Brick pavers

Stone pavers

Colored concrete – scored or patterned per City approval

Decomposed granite or other gravel material with binder (limited to appropriate plaza or other pedestrian areas where consistent maintenance is assured)

Where pedestrian routes intersect vehicular access routes, the material of the pedestrian route should be different from the vehicular paving material.

All pedestrian circulation routes should be adequately illuminated for safety and use (Section 1.7 Site Lighting).

## 1.10 Plazas/ Public Spaces

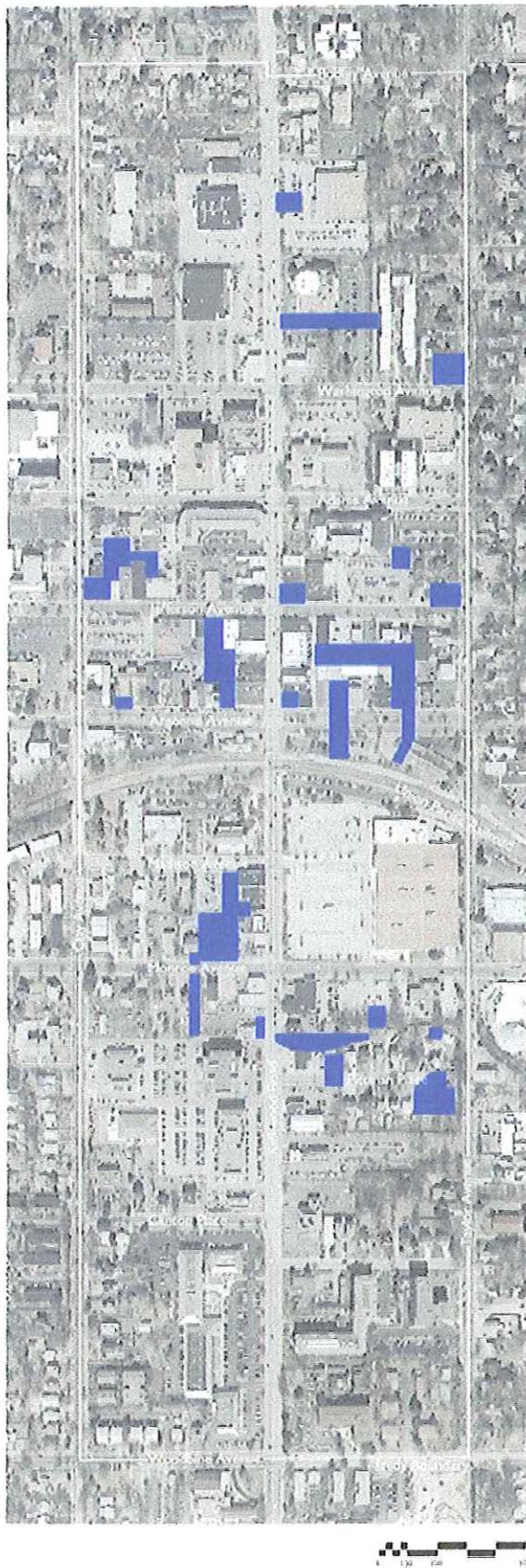
Plaza/Civic Spaces are meant to enhance the downtown by providing pleasant, public living, working and gathering environments. Provisions for benches, landscaping, water features, and lighting should be provided. The creation of such spaces are intended to establish a public identity for the City, create orientation elements for those visiting the downtown, and allow for potential physical connections between public and private enterprises.

The creation of private plaza space is strongly encouraged for outdoor use by the development for dining, vending operations, programmed events and gatherings, and other activities allowable by the City.

Public art variations are also encouraged to be developed in and around these areas. Artworks can exist in many forms and provide enhancements to the surrounding spaces. Developers are encouraged to also work with the City in coordinating the location of art pieces along the downtown streetscapes.



**Examples of Public and Private Plaza Spaces**



Plaza or Civic Spaces

Recommended Public  
Plazas/Civic Spaces

**Figure 6 Recommended Plazas/ Public Spaces**

## **Section 1.11 Planting Design Standards**

Landscaped areas should accent the overall natural topographic conditions of the site and the character of the development on the site. Within landscaped areas provisions for lighting, benches, and other pedestrian related amenities should be made in order to create an overall system of landscape spaces accented by individual "garden-like" pieces.

Trees recommended by the Kirkwood Urban Forestry Commission on their latest "Street Tree Selections for Kirkwood" are suggested to be used within the downtown area in combinations of at least two, but not more than three species for private landscape or street tree development (on an individual site or street basis).

(Amended by Planning and Zoning August 17, 2005, per recommendation by Urban Forestry Commission)

## **1.12 Site Signage**

The plan will be used to regulate the types and use of site and parcel signs in the downtown area. The purpose of regulating signage in the downtown is to create an image and identity for each parcel development that is consistent, compatible, and harmonious with surrounding areas and developments.

All text and graphic information displayed on sign faces should match or be compatible with the primary accent color on the building.

Unless otherwise noted in this section, all background colors used on signs should match or be compatible with primary accent colors used on building elevations, and should be approved by the City.

Exterior neon signage and lighting that is visible from the public streets shall not be allowed, without approval by the City regardless of scale, or time of installation.

Box signs, "cut-sheet", or other sign types where the entire sign surface is illuminated are prohibited. Where text or graphics appear, each character should be illuminated independently.

Monument signs (one per parcel) should identify parcels with the following criteria:

All monument signs should be 5' maximum in height (inclusive of base), 8' maximum in length, minimum 18" in width, and maximum 24" in width.

All monument signs should have a brick masonry or stone base that is coordinated with the primary finish material used in the project building elevation.

Parcel monument signs may include the name or graphic identification of the project or development as a header over tenant names. Such graphic or text should not exceed 18" in height.



#### **Monument Sign Examples**

Pole and/ or pylon signs are prohibited.

Parking direction and information signage such as "no parking – tow-away zone", etc. that is not regulated by state or federal law, should be of a consistent color that is approved and used by the City for street sign marking purposes.

Allowable temporary signs include:

- Construction walls/ barricades
- Construction signs
- Lease, Real Estate and other signs required by law

Flagpoles should be a maximum of 30' in height. Flags should be no larger than 3' x 9' in area. The number of flagpoles should generally be limited to two per parcel.

Permanent banners and pennants may not be used on the building exterior or site.

Special occasion banners, balloons, or pennants may be temporarily erected for special events only with prior approval by the City.

### **1.13 Fountains and Water Elements**

The development of fountains and other water features in the downtown are encouraged to create community identity elements and should be combined with plaza/ public spaces, private pedestrian spaces, and other open space conditions where possible. Scale, form, materials and character of the fountains should enhance the surrounding spaces and developments and should follow the architectural guidelines that are defined in section 2.0 Building Design.



**Fountains and Water Elements Used to Establish Community and Project Identity**

### **1.14 Service, Loading and Utilities**

Loading areas should be accommodated entirely on-site for each parcel.

Parallel parking space for delivery vehicles should be provided along service drives or in specially designated courts or loading areas.

Loading docks and trash storage should not be located along street frontages and should be screened from view with landscape or architectural elements designed as part of the building structure. Service elements such as loading doors should be integrated with the building elevation design so as to minimize the visual impact of such elements.

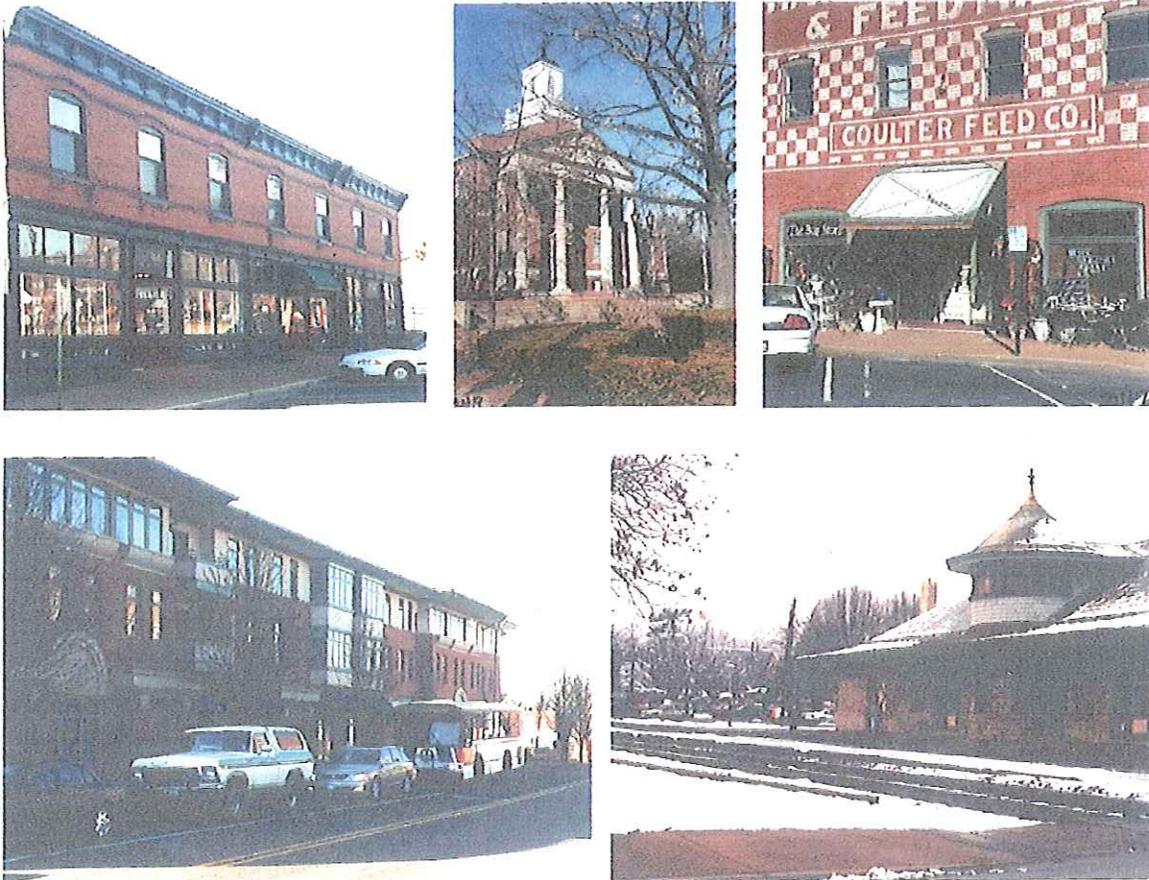
All new installations and replacement of existing exterior utilities such as water, gas, sewerage, electrical, and communication lines should be installed underground.

Where potentially visible from a public street, all mechanical equipment, utility meters, storage tanks, air conditioning equipment, and similar equipment should be screened from view by landscaping (see buffers) or architectural elements integrated into the structure.

## 2.0 Building Design

The fundamental character of the architecture of the downtown Kirkwood is identified as having simple, authentic form, that is appropriately scaled and situated, with consistent, high quality materials, and refined building details and elements.

The purpose of section 2.0 is to maintain these existing qualities and to create new design conditions to ensure that each new building, addition, and renovation contributes to the rich and intelligible visual and environmental heritage of the Downtown area.



**Existing Context of Traditional Materials and Simple Building Forms**

## 2.1 Building Massing, Scale

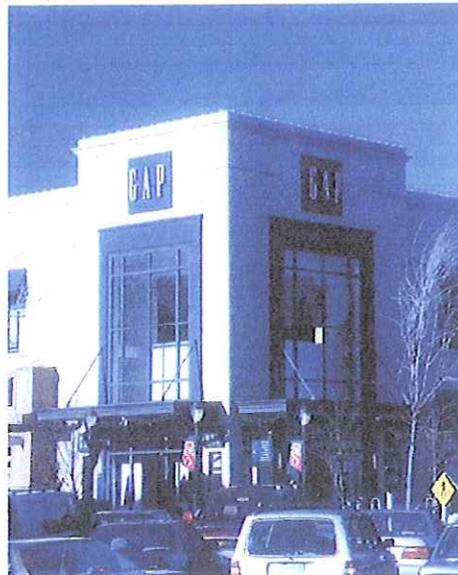
Each building shall have simple, well-proportioned, volumetric form— a simple form that is consistent with the building use, and its construction materials and methods.

Any parapets shall be designed as integral to the mass of the building. Two-dimensional projections shall not be allowed above the building roof-line.

No colonnades of one story in height shall be permitted to project beyond the face of buildings. Loggias and other architectural elements may be created by recessing into the mass of the building, but shall not be applied decoration or additive components.



**Examples of Appropriate Building Massing and Scale**



**Building Massing at Major Entry**

The nature of the openings in the façade, primarily doors and windows, their location, size, and proportion, shall be compatible with the illustrative examples shown in section 2.0.

Building scale should reflect the particular use of the building. Its size and massing should be compatible with existing, adjacent buildings. Special attention shall be given to regulating lines and proportions established by existing buildings in order to create or maintain a coherent architecture assemblage of building forms in any given area. Regulating lines and proportions include significant formal elements such as roof or cornice lines, fenestration heights and proportions, and entry size. Building height within areas designated for commercial land use shall be a minimum of 25' in height on all sides.

Massing will be regulated on street, parking and pedestrian way frontage by an architectural articulation of the façade or form of the building so that the horizontal massing does not exceed 100 linear feet and appropriately less in areas that have an existing smaller scale. Buildings shall be thus articulated with entries, courtyards, or arcades or other recessive elements visually segmenting the form of the building.

## **2.2 Consistent Quality Materials**

To create a coherent and harmonious environment in the Downtown area, buildings shall be constructed with materials and colors compatible with their specific context. Building construction shall be of high quality and durable materials. Building materials shall be used in a consistent manner on the exterior of the building. Materials and colors on all exposed façades shall be given equal value.

The colors of exposed exterior components of a building are to be of low reflectance, subtle, neutral, or earthtone colors. High-intensity or metallic colors are not recommended.. The different colors and any material patterns (such as a brick or stone color mix or pattern) should display a subtle color range with the color saturation and brightness not to vary more than 20%.

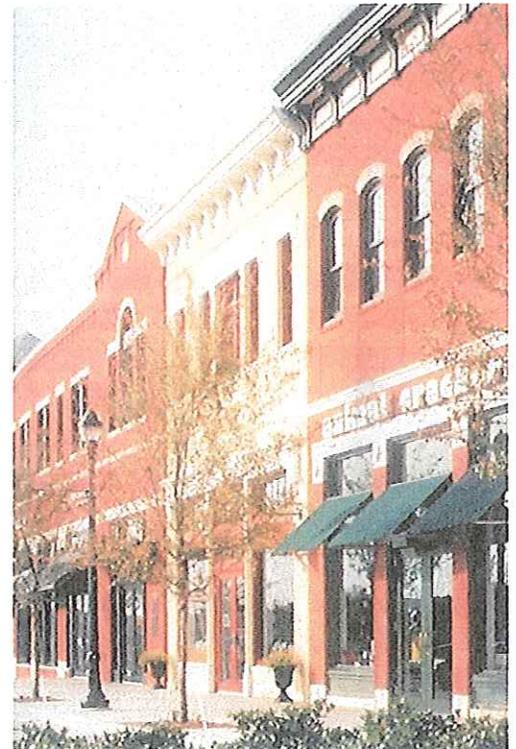
Maintenance painting is required to match pre-approved colors. Proposed changes in color for any existing building shall require submittal of color samples for review and approval by the City.

**Recommended** visible exterior construction materials are:

- Brick, natural clay
- Stone, natural
- Non-rusticated synthetic stone
- Finished and precast concrete
- Plaster stucco
- Glass (must be clear or nearly clear, extent not to exceed 50% on any façade)
- Metal for detailing
- Copper
- Slate

**Allowed** in certain circumstances when properly used:

- Vinyl siding, or vinyl wrapped details (Residential)
- EIFS - Exterior Insulation Finishing Systems
- Rough sawn or treated lumber
- Proprietary painted metal roof systems
- Aluminum or vinyl gutters and downspouts
- Concrete block
- Tile



**Examples of Consistent Quality Materials, Massing and Articulation of Frontage**

## 2.3 Building Details

Human scale, unique detailing of the building's system of architectural elements – entrances, windows, roof and base of building – is required. Particular emphasis shall be placed on components that are used directly by people; these components deserving of unique and crafted detailing include doorways, display windows, and seating. Such elements shall be fully detailed as part of the documents submitted for review by the City.

Designs of new buildings may be contemporary or may reference design motifs found in the downtown district. Such design should be compatible with the size, scale, color, material and character of the surrounding buildings, district and environment.

Duplicating the exact form, material, style, and detail of a historic building in the design of a new addition so that the new work appears to be part of the historic building, is not recommended. All buildings and structures shall be recognized as products of their own time. Referencing elements of Kirkwood's existing building styles is appropriate.

Commercial building designs that are obviously national or regional prototypes shall not be allowed.

Where existing historic buildings are to be included in redevelopment proposals, the owner shall refer to the United States Department of Interior Standard for Rehabilitation for guidance.



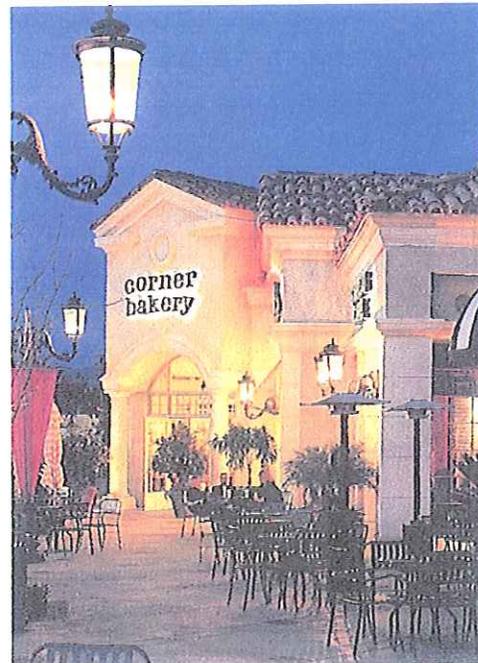
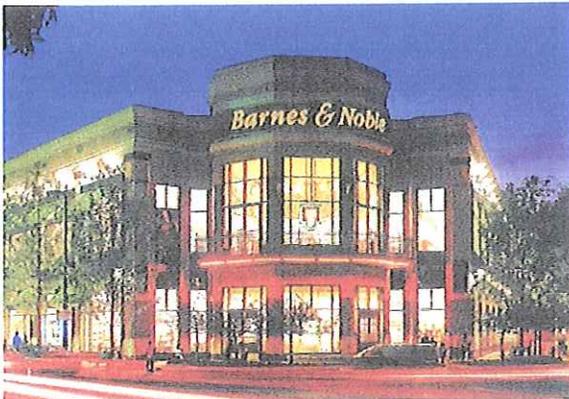
**Examples of Positive Building Details**

## 2.4 Lighting – Interior/ Exterior

Building illumination and architectural lighting should be indirect in character (no light source visible). Indirect wall lighting, overhead down-lighting, or interior illumination, which spills into the landscape, is encouraged. Architectural lighting should articulate and animate the particular building design as well as provide the required functional lighting for safety and clarity of pedestrian movement. Lighting should highlight, complement, and reinforce landscape and architectural design as focal points of interest.

Lighting should not impact adjacent residential areas or traffic movements. Sky light pollution should be minimized.

Non-color corrected mercury vapor lighting is prohibited. Sodium lighting is prohibited.



**Examples of Positive Building Lighting**

## 2.5 Signage/Building Addresses

Unique building signage is encouraged within the following limitations and as approved by the City.

Signage on a building is strictly limited to street address and building or business identification; additional advertising or depictions of available services is not permitted. Building signage and street addresses on the façades of buildings shall be individual cast or cut metal letters that are consistent with the architecture of the building. Anchor tenants shall have signage heights reviewed on an individual basis. Signage for remaining tenants shall be limited to 8 inches in height and located on a recessed surface of the building. Signage color for non-anchor tenants will be limited to one color per building or building group. Covered pedestrian walkways shall be allowed additional signage suspended from the ceiling or by plaques recessed in walls. Signboards at covered walkways shall be limited in size to a maximum of 12" x 24" and shall be consistent in color, size, and material within each building or building group. Information signage in windows shall be permanently affixed to the glass by individually painted or vinyl letters and limited in size to 3" letter height and limited to 10% of the individual window area. Awning signage shall be integral to the awning and limited in size to 6" height. Backlit, interior illuminated, individually lit can letters, and billboard signs are not permitted.

Proposals for any other temporary signage, banners, or commercial displays shall be submitted for approval by the City on an individual basis.



**Examples of Building Signage**

## 2.6 Mechanical and Service Screening

In order to unify and support the Downtown area's visual character, storage, service and truck loading areas, utility structures and mechanical equipment on the ground or roof should be screened from public view. Any emission of noise, vapor, heat or fumes should be mitigated.

Consideration should be given to developing common service courts at the interior of blocks.

Garbage, recycling collection and utility areas shall be enclosed and screened around their perimeter by walls minimum of 8' in height and constructed of materials consistent with the rest of the building. Water meters, gas meters, electric meters, ground-mounted mechanical units, and any other similar structures should be hidden from public view or screened with approved construction materials. Fences designed for privacy shall be constructed of brick, stone, architectural stucco, concrete. The use of wood and metal is limited to material for gates. Chain-link, plastic or wire fencing is prohibited. Mechanical units, utility equipment and telecommunication receiving devices located on the roof shall be grouped together and incorporated into the roof design or thoroughly screened by a parapet integral with the building.

## 2.7 Accessibility

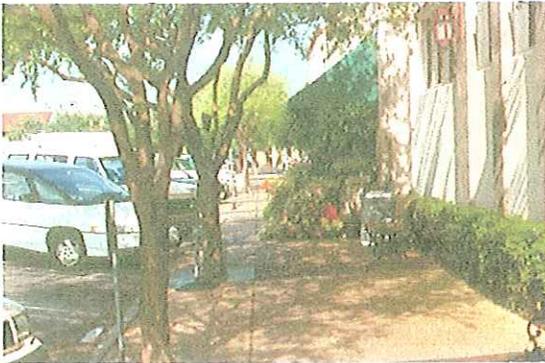
All buildings shall conform to the ADA (Americans with Disabilities Act) Standards for accessibility. ADA accessible entries should be thoughtfully integrated into the design of the building and not separated from main building entries.



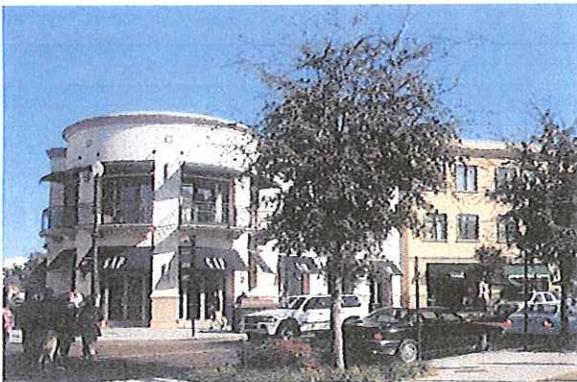
**Accessible Ramp Integrated with Building Entry**

## 2.8 Relationship of Building to Parking and/ or Street

All buildings should be designed to create a strong physical relationship with their street frontages. Every building shall have a clearly defined, highly visible entrance with distinguishing features oriented toward the street-side of the building, connecting with the street sidewalk. Additional entrances may be provided and oriented toward parking courts or pedestrian paths. Smaller retail stores that are a part of a larger principal building shall have display windows and separate outside entrances.



**Examples of Buildings with Strong Relationship to Streets**



**Examples of Retail with parking garages**

In order to create a strong building to street relationship, individual property owners should consult with adjacent owners to coordinate the location of development activities and resources such as shared service alleys, parking structures, etc.

## 2.9 Structured Parking

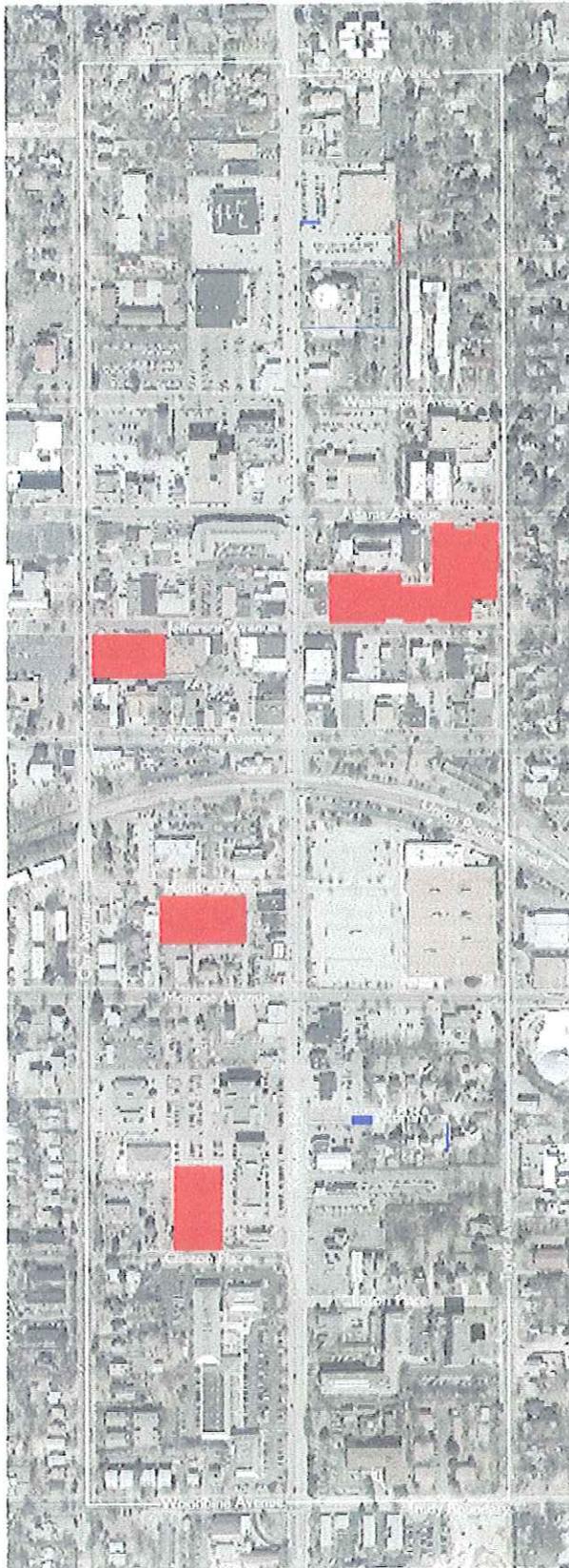
Parking structures are critical to a successful evolution of urban form, variety and intensity of use, and economic vitality within the downtown area. Conversely, the elimination of surface parking areas needs to be encouraged through shared parking arrangements and other incentives. Depending on where future development or redevelopment occurs within the downtown area and its associated parking requirements, parking structures are recommended in four potential locations as illustrated in Figure 7. It is important to note that these locations do not represent an intent or commitment to develop any specific structured parking at these locations. Any parking structure development shall be based on a thorough needs assessment and detailed design process. Following such analysis, the City could support the development of parking structures at these locations or other locations in conjunction with specific development proposals.

If a parking structure development is proposed, the UD plan requires that retail or other commercial uses occupy 70% of the frontage of the ground floor that faces the primary street where the structure is located. This retail or commercial use shall be fully accessible from the public way with primary entries facing the sidewalk. The building elevation not occupied by retail or commercial uses or entries should be screened in a manner that is integrated with the building architecture, materials, and overall concept. Any parking structure with frontage on a residential street, including Taylor, Clay, and Bodley Avenue, shall have a minimum setback of 20 feet on such streets and shall be fully screened with a double row of major trees, and full shrub hedges which shall be a minimum of 6 feet in height.

Ground floor areas not required for retail or commercial use, or for ingress and egress should be articulated and designed to create a harmonious appearance with the buildings that they serve and their immediate surroundings.



**Examples of Parking Garages**




 Locations for  
 Parking Structure

**Recommended Parking  
 Structure Locations**

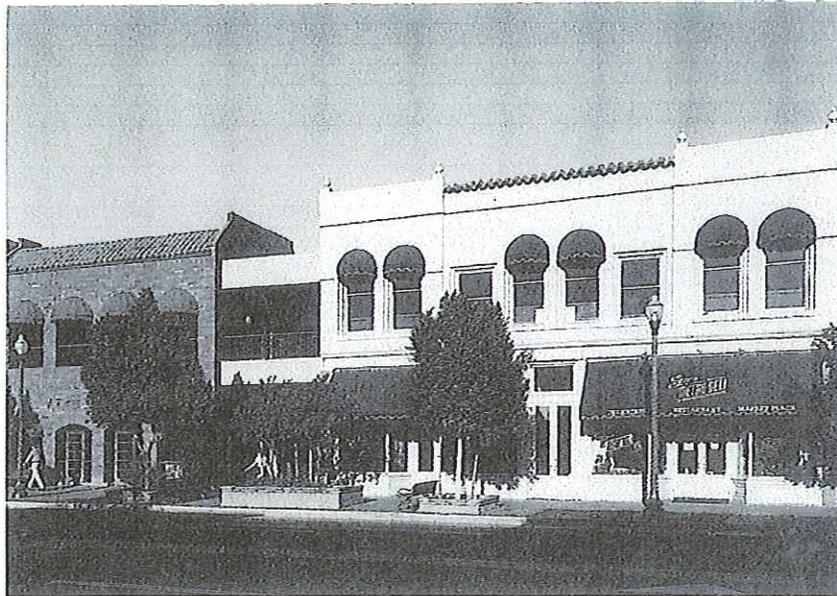
**Figure 7 Potential Parking Structure Locations**

## 2.10 Historic Preservation

All buildings within the downtown area that are classified as Landmarks, or otherwise carry significant historic value as determined by the Landmarks Commission shall conform to all requirements established for such properties by the Commission. Such buildings shall be preserved under the guidelines set forth by the City, and shall be maintained and/or redeveloped in a manner that sustains the intrinsic historical value of the structure.

All structures and sites determined by the City to conform to Landmarks status shall be reviewed for conformance to both the Landmarks Commission and UD Plan requirements. Such properties shall in no way be exempted from either set of requirements as adopted by the City. Modifications to any Landmark shall first be reviewed for conformance to the current Landmarks Commission requirements, and upon receipt of approval by the Commission, may then be submitted for review by the City for conformance to the UD Plan requirements per the process outlined in this document.

### Landmark Property and Associated New Development



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**APPENDIX I, DOWNTOWN TRANSPORTATION PLAN**  
**Appendix II, Public Improvement Funding Alternatives**

Appendix I and Appendix II are incorporated into the Downtown Urban Design Plan by reference and are available under separate cover.

A transportation study was undertaken in support of the goals and objectives established in the Downtown Urban Design Plan. The study is not intended to provide detailed engineered solutions to Kirkwood's downtown transportation issues, rather the focus is on identifying current and future transportation challenges and practical techniques to address them. The following transportation study findings and recommendations are critical elements that will assist in maintenance of a sustainable and livable downtown area – one which conforms to Kirkwood's vision of a pedestrian friendly, and fully accessible urban core.

These recommendations are intended for implementation as the downtown area evolves within the framework of the UD Plan guidelines and requirements. Implementation triggers for transportation recommendations are described in concept within the study, and mechanisms for such improvements are further outlined in *Appendix II, Public Improvement Funding Alternatives*. As recommended in the study, all future development proposals within the downtown area should include both a Traffic Impact Study and a Parking Study to be submitted for the City's review. All transportation related improvements – including both public and private development – should be carried out in a manner that fully supports the requirements of the UD plan and all applicable design guidelines.